



APPLICATION FOR PERMIT TO CONSTRUCT ACCESS DRIVEWAY FACILITIES WITHIN THE CITY OF BULVERDE RIGHT-OF-WAY

DATE OF APPLICATION: _____ PERMIT # _____

PROJECT ADDRESS: _____

SUBDIVISION: _____

HOME OWNER: _____ PHONE: _____

CONTRACTOR: _____ (MUST BE REGISTERED WITH THE CITY)

CONTRACTOR PHONE: _____

CONTACT NAME: _____

THE CITY OF BULVERDE HEREBY AUTHORIZES THE ABOVE APPLICANT TO CONSTRUCT DRIVEWAY FACILITIES WITHIN THE RIGHT-OF-WAY OWNED BY THE CITY OF BULVERDE, PROVIDED THE APPLICANT AGREES TO THE FOLLOWING CONDITIONS:

NO WORK IS TO BE STARTED ON THE CITY ROAD RIGHT-OF-WAY UNTIL THE PERMIT APPLICATION HAS BEEN RECEIVED, APPROVED, AND A PERMIT ISSUED BY THE CITY OF BULVERDE.

- 1. Applicant shall attach a dimensional plan showing the following:
a. The location of the proposed driveway showing the distance from the proposed driveway to the lot lines.
b. The location of existing curb cuts or driveway openings on the same lot.
c. The location of existing or proposed buildings, curbs, sidewalks, trees, poles, public utilities or other objects which might affect work.
d. Description of culvert size and roadway materials used.
e. Method of maintaining the integrity of the positive drainage on the City of Bulverde's right-of-way.
2. All Construction and materials shall be subject to inspection and approval by the City of Bulverde. A Pre-Pour inspection must take place before driveway is poured. A Pre-Pour inspection constitutes a Final Inspection. Failure to complete a Pre-Pour inspection shall result in deposit not being refunded.
3. Maintenance of facilities constructed hereunder shall be the responsibility of the applicant. The City of Bulverde reserves the right to require any changes, maintenance, or repairs as may be necessary to provide protection of life or property on or adjacent to the roadway. Changes in design will be made only with approval from the City.
4. The applicant shall hold harmless the City of Bulverde and its duly appointed agents and employees against any action for personal injury or property damage sustained by reason of the exercise of this permit.
5. The applicant shall not erect any sign or structure, on or extending over, any portion of the roadway and/or right-of-way, except for a mailbox.

This application shall not be valid until applicant signs the statement in which he/she agrees to comply with the conditions herein, submits the one hundred twenty-five dollar (\$125.00) application fee and a five hundred dollar (\$500.00) deposit to the City of Bulverde.

I (We), the undersigned, hereby agree to accept and comply with the terms and conditions set out in this permit application for the construction of a driveway approach within the right-of-way of the City of Bulverde.

Applicant's signature

Applicant's printed name

Approved by

Date approved

1: DRIVEWAY APPROACH DETAIL

TO BE COMPLETED BY APPLICANT. - N.T.S.

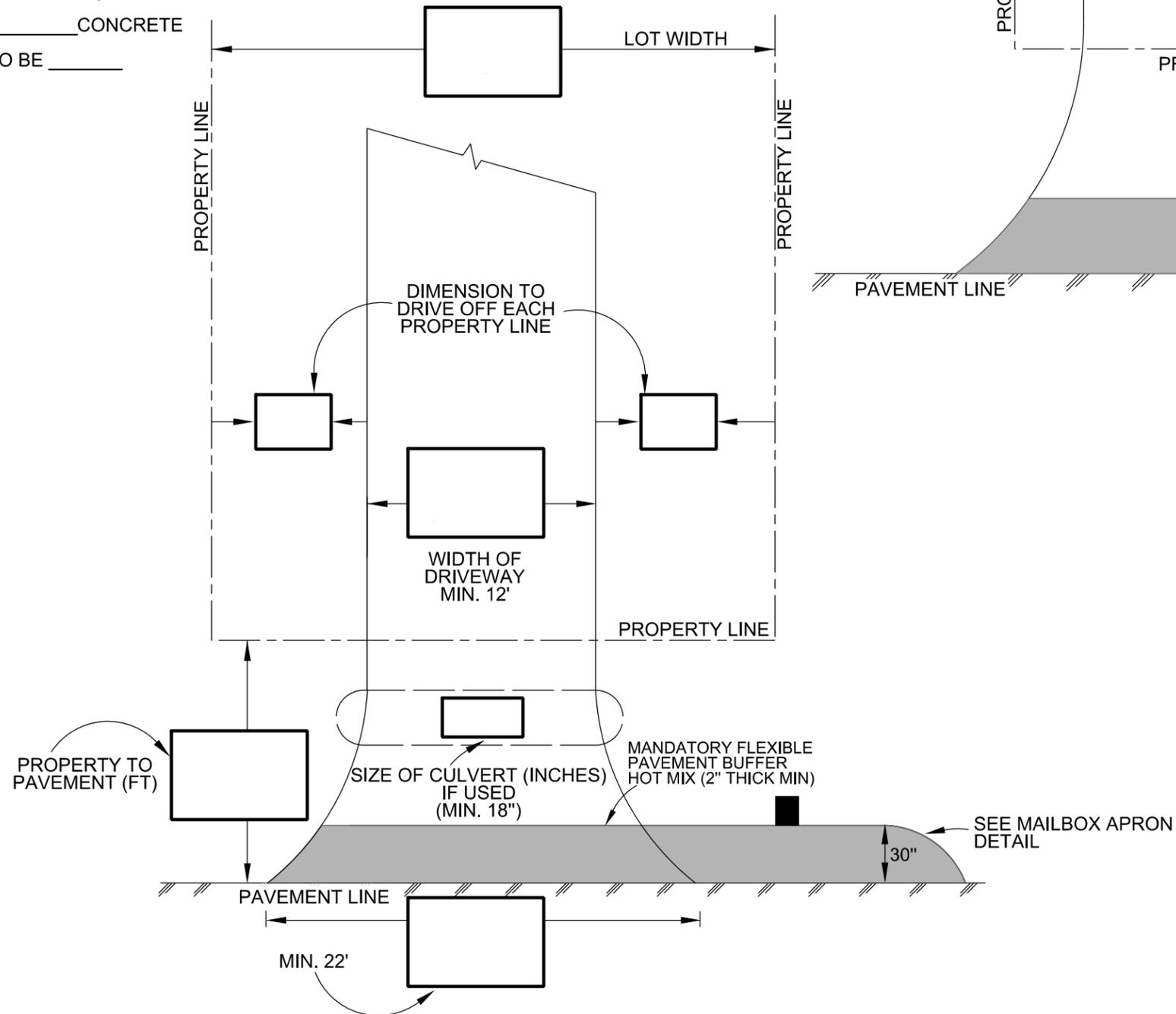
1) DRIVEWAY TYPE: (CHECK ONE)

- TYPE 1: ASPHALT DIP-TYPE
- TYPE 2: CONCRETE DIP-TYPE DRIVEWAY
- TYPE 3: DRIVEWAY WITH CULVERT
- TYPE 4: RESIDENTIAL - HIGH DENSITY
- TYPE 5: TEMPORARY DRIVEWAY/CONSTRUCTION ENTRANCE

2) MATERIAL TO BE USED: (CHECK ONE)

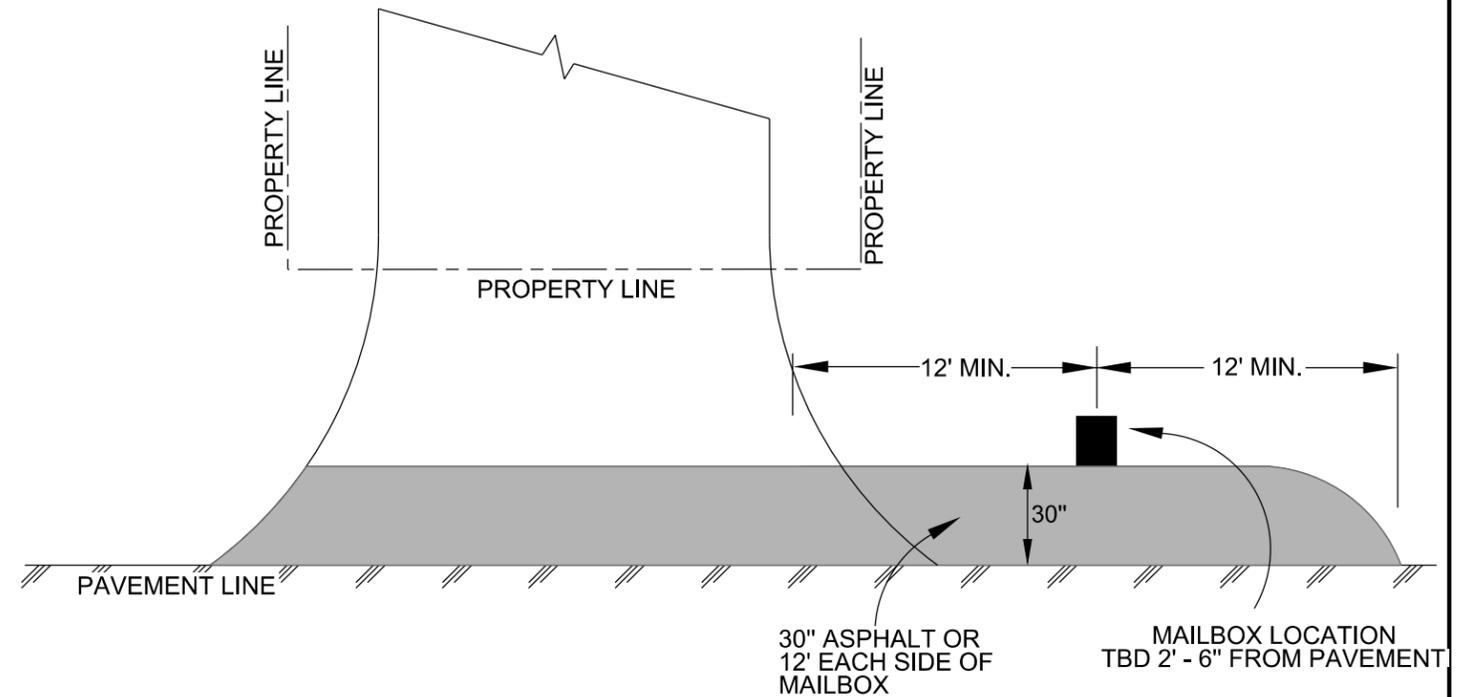
ASPHALT OR CONCRETE

3) CULVERT SIZE (IF USED) TO BE _____
(MINIMUM 18")



2: MAILBOX APRON DETAIL

N.T.S.

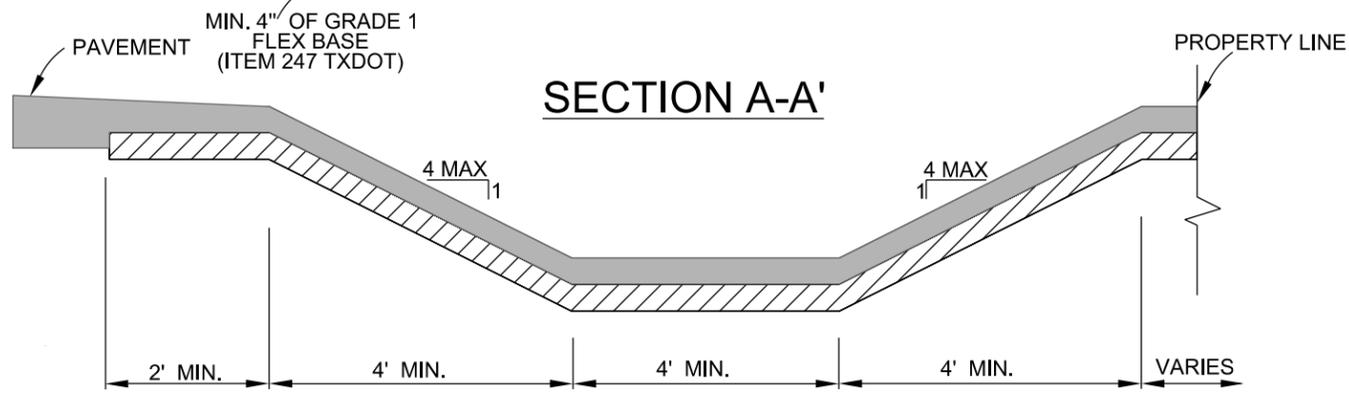
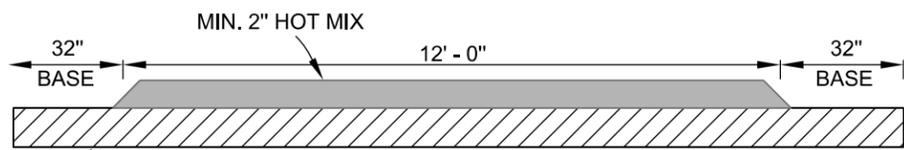
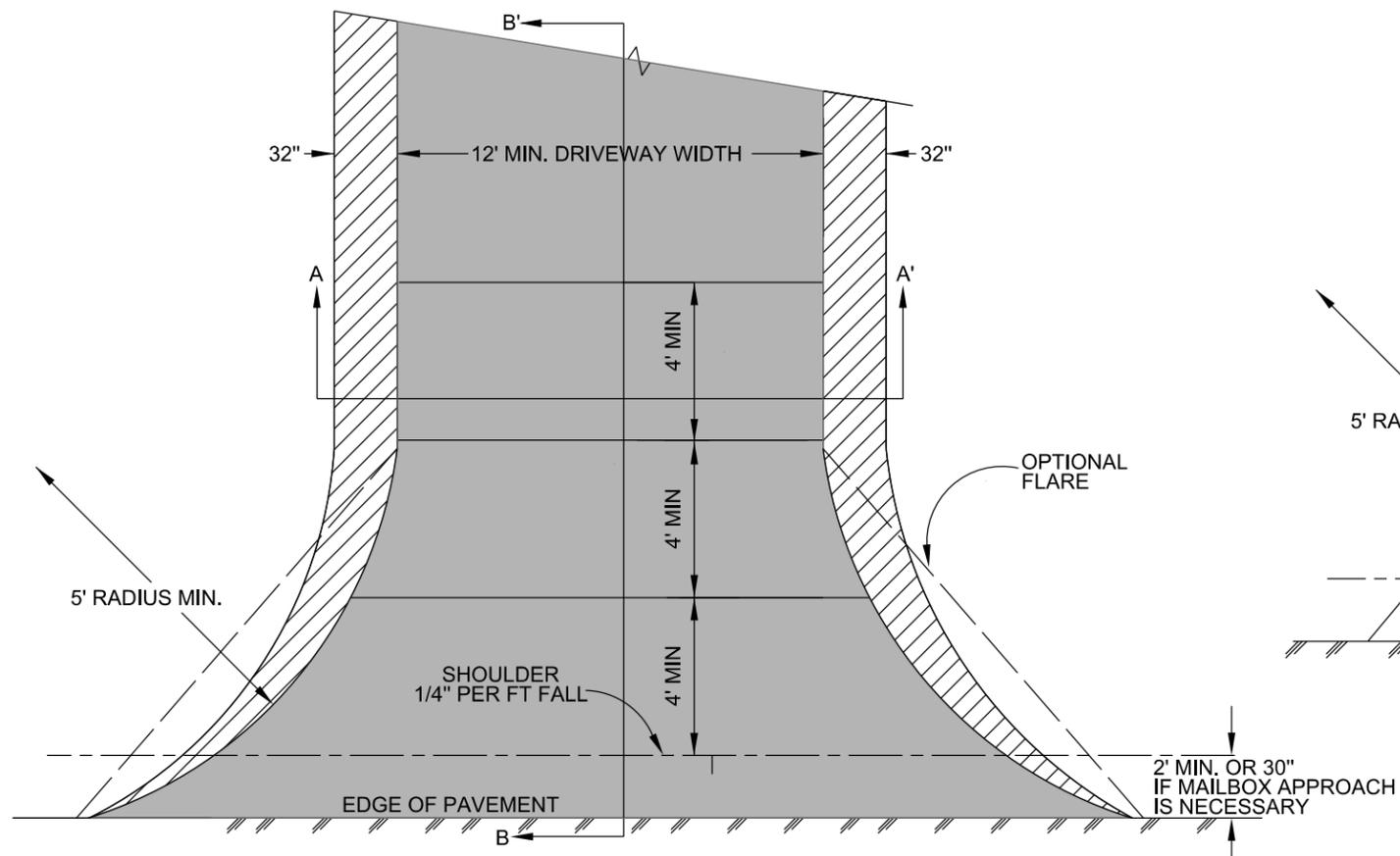


CITY OF BULVERDE, TX	
DRIVEWAY STANDARDS	
	SHEET 1 OF 5

TYPE 1: ASPHALT DIP-TYPE DRIVEWAY

N.T.S.

NOTE: THIS METHOD MAY BE USED IF DITCH GRADE IS 0% TO 3%.

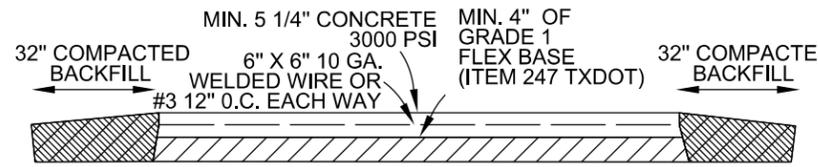
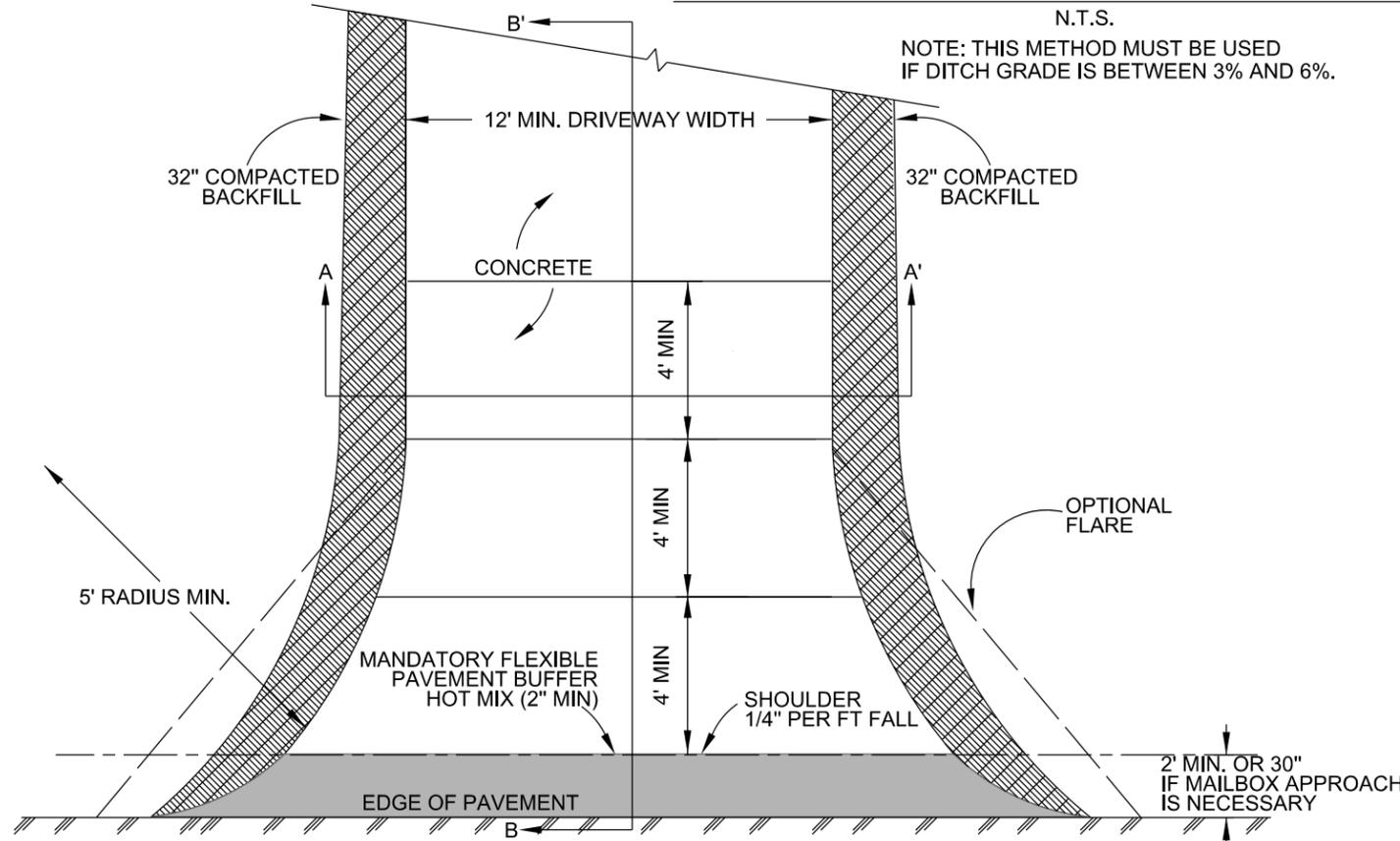


SECTION B-B'

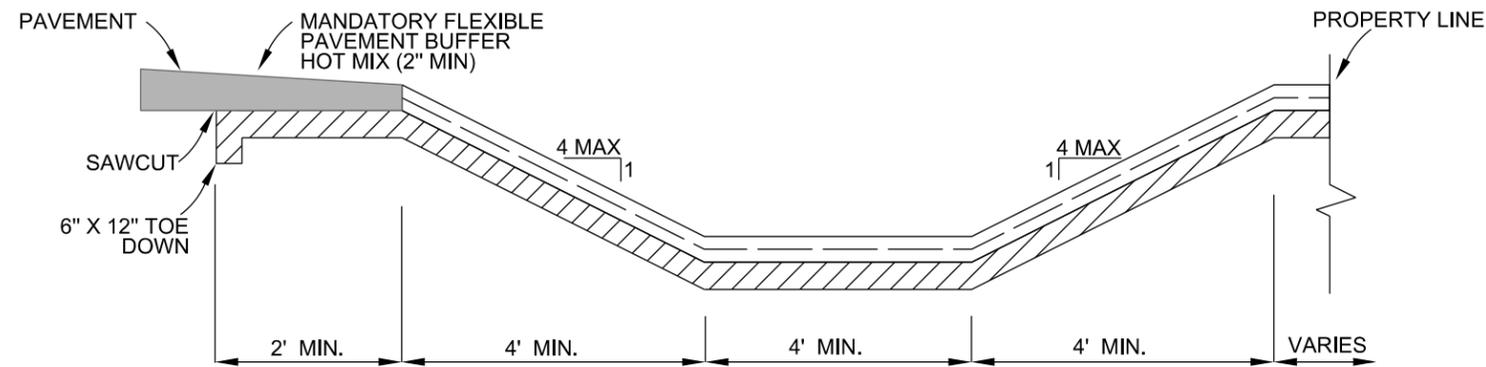
TYPE 2: CONCRETE DIP-TYPE DRIVEWAY

N.T.S.

NOTE: THIS METHOD MUST BE USED IF DITCH GRADE IS BETWEEN 3% AND 6%.



SECTION A-A'



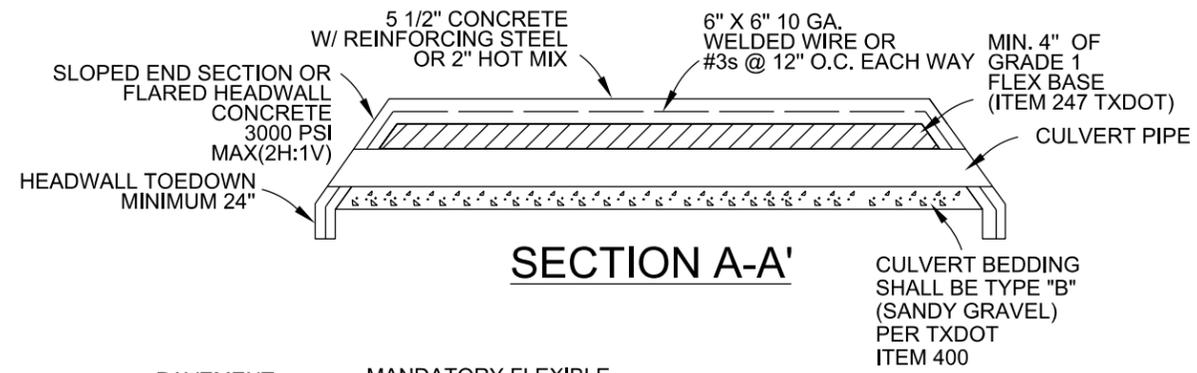
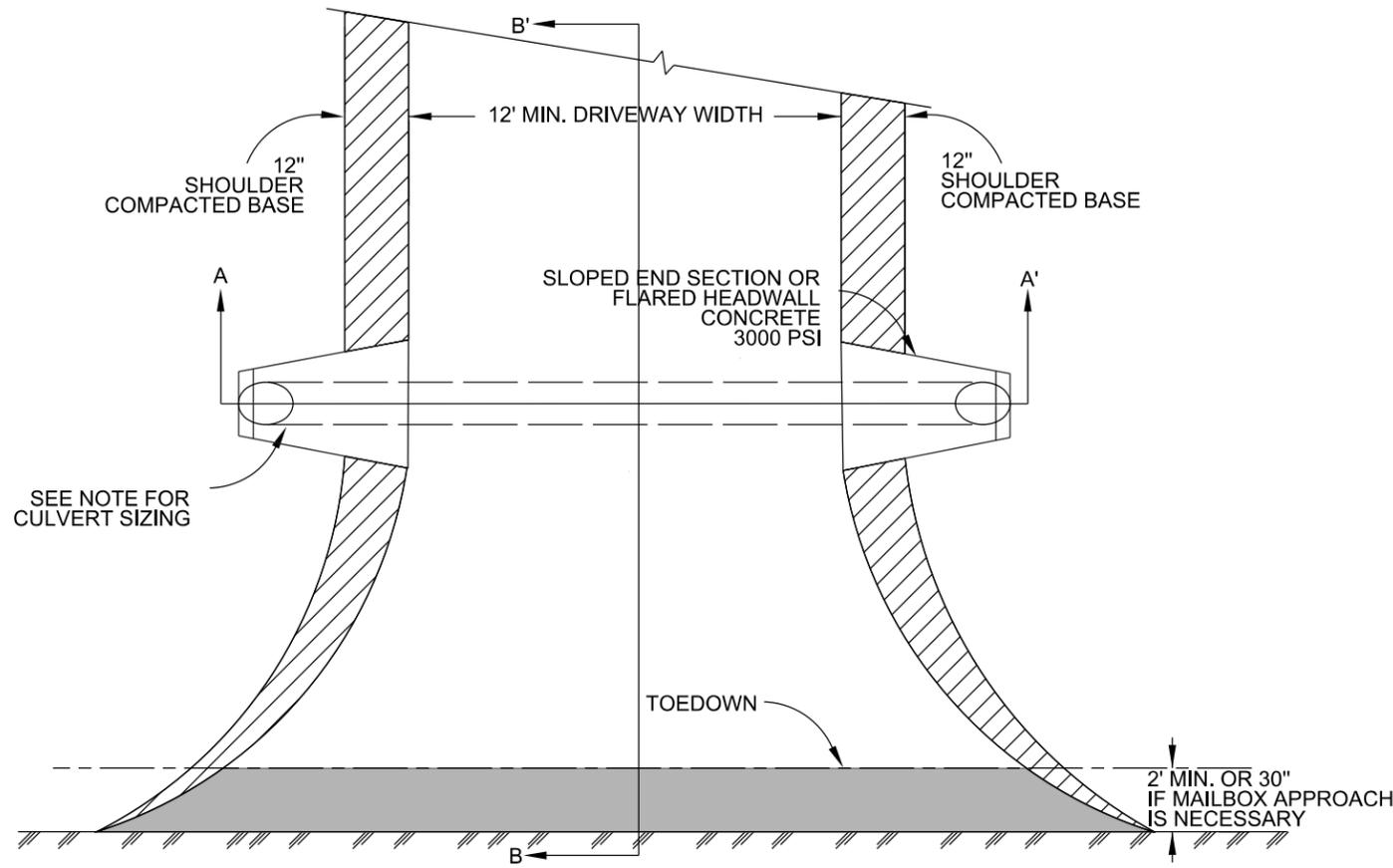
SECTION B-B'

CITY OF BULVERDE, TX	
DRIVEWAY STANDARDS DIP-TYPE	
	SHEET 2 OF 5

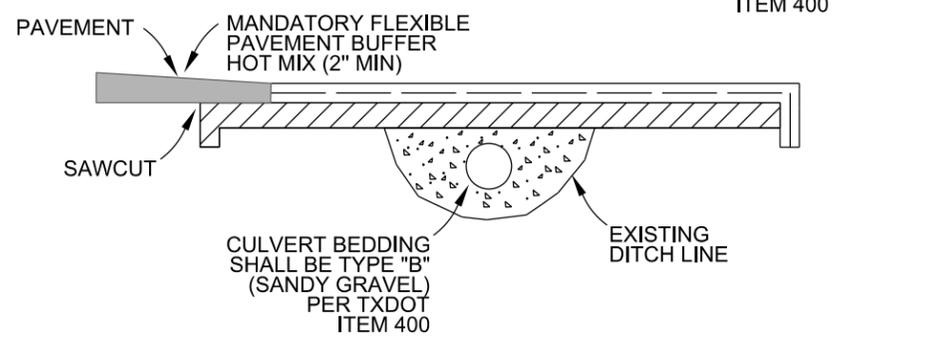
TYPE 3: DRIVEWAY WITH CULVERT

N.T.S

NOTE: CULVERTS ARE REQUIRED IF DITCH GRADE IS 6% OR GREATER



SECTION A-A'

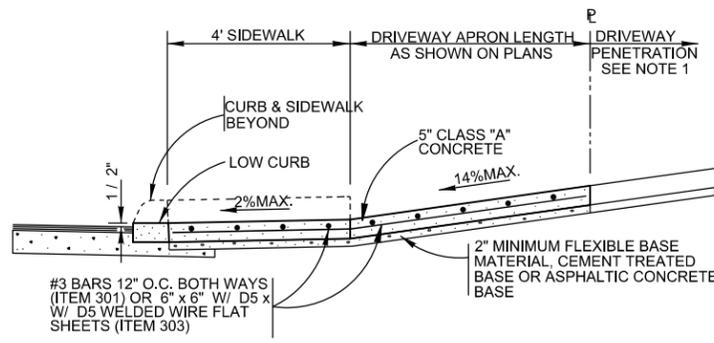


SECTION B-B'

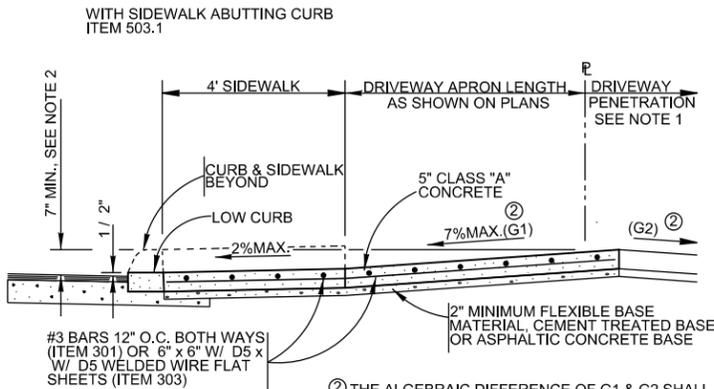
NOTES:

1. BASE MATERIAL ON CITY R.O.W. SHALL BE A MINIMUM THICKNESS OF 4" GRADE 1 FLEX BASE (ITEM 247 TXDOT) COMPACTED TO 95% AND EXTEND 12" MINIMUM BEYOND WEARING COURSE ON EACH SIDE.
2. SIZE OF CULVERT SHALL VARY DEPENDING UPON LOCATION TO CONTROL RUNOFF THRU CULVERT.
3. EDGE OF CITY STREET SHALL BE SAW CUT ADJACENT TO NEW DRIVEWAY.
4. MATCH SLOPE GRADE FOR CULVERT END CUTS (2H:1V MAX).

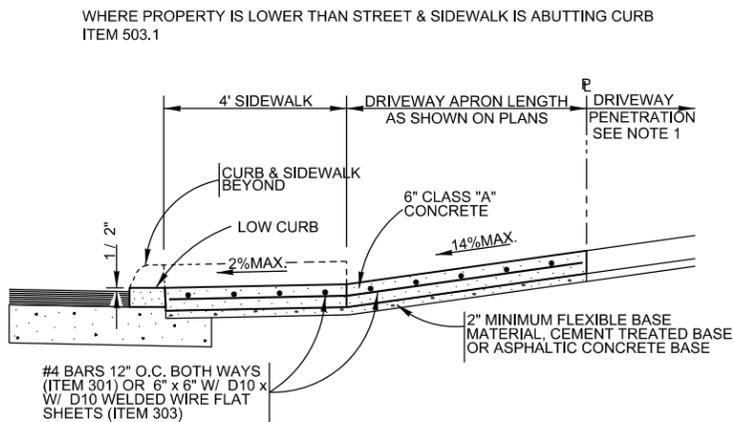
CITY OF BULVERDE, TX	
DRIVEWAY STANDARDS CULVERT TYPE	
	SHEET 3 OF 5



TYPICAL RESIDENTIAL DRIVEWAY SECTION
WITH SIDEWALK ABUTTING CURB
ITEM 503.1



TYPICAL RESIDENTIAL DRIVEWAY SECTION
WHERE PROPERTY IS LOWER THAN STREET & SIDEWALK IS ABUTTING CURB
ITEM 503.1



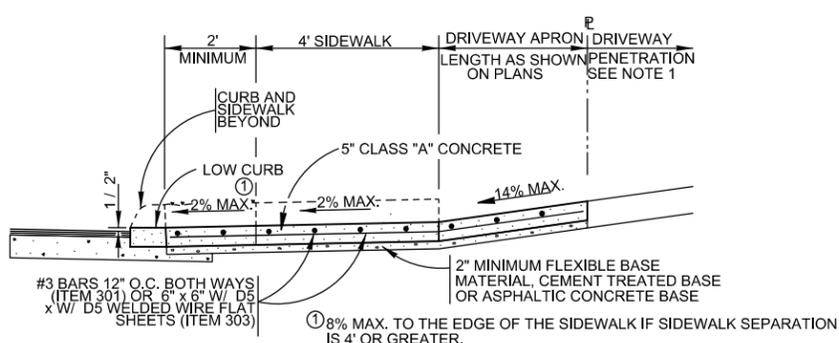
TYPICAL RESIDENTIAL DRIVEWAY SECTION
WHERE PROPERTY IS LOWER THAN STREET & SIDEWALK IS SEPARATED FROM CURB
ITEM 503.1



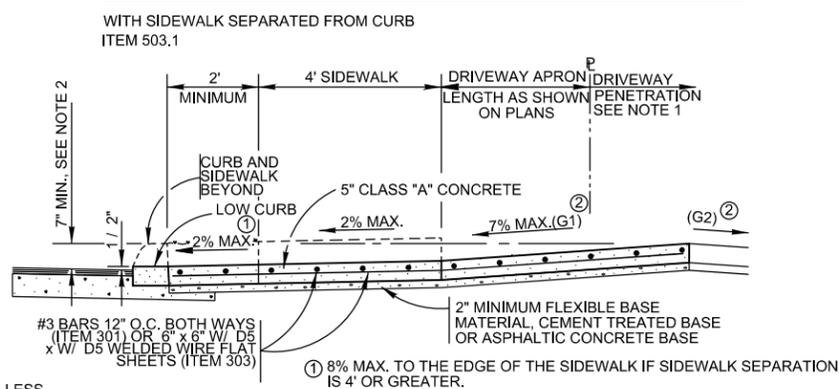
TYPICAL COMMERCIAL DRIVEWAY SECTION
WITH SIDEWALK ABUTTING CURB
ITEM 503.2



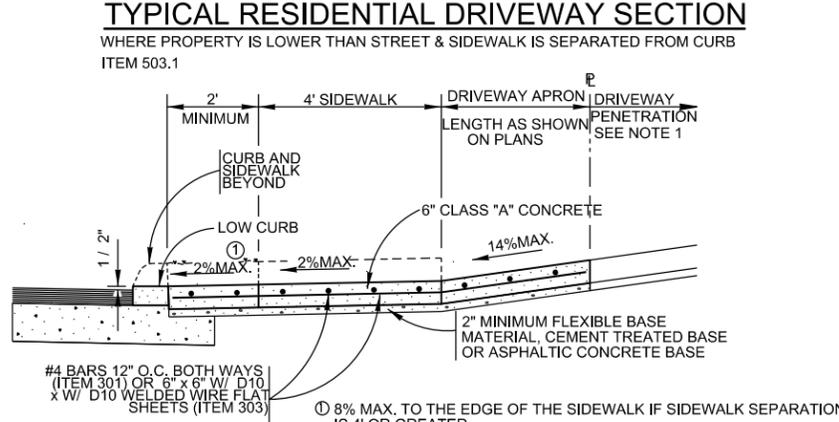
TYPICAL COMMERCIAL DRIVEWAY SECTION
WITH SIDEWALK SEPARATED FROM CURB
ITEM 503.2



TYPICAL RESIDENTIAL DRIVEWAY SECTION
WITH SIDEWALK SEPARATED FROM CURB
ITEM 503.1



TYPICAL RESIDENTIAL DRIVEWAY SECTION
WHERE PROPERTY IS LOWER THAN STREET & SIDEWALK IS ABUTTING CURB
ITEM 503.1



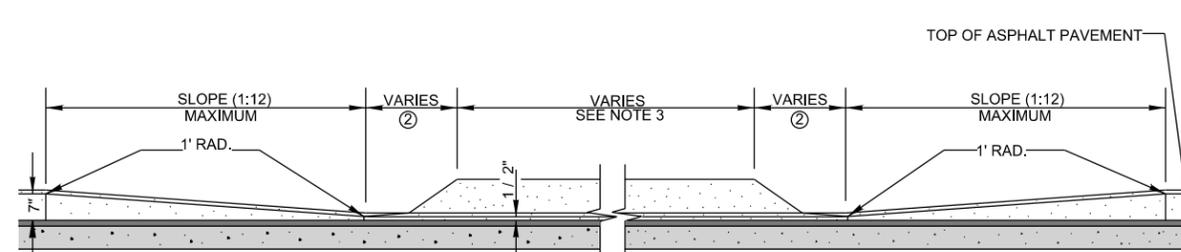
TYPICAL RESIDENTIAL DRIVEWAY SECTION
WHERE PROPERTY IS LOWER THAN STREET & SIDEWALK IS SEPARATED FROM CURB
ITEM 503.1



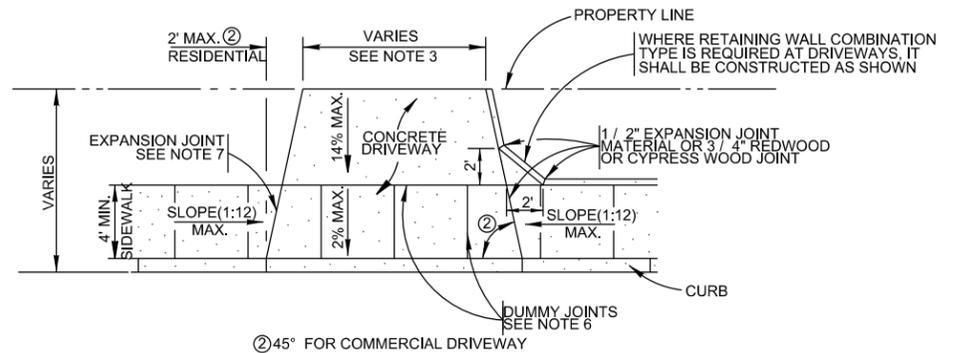
TYPICAL COMMERCIAL DRIVEWAY SECTION
WITH SIDEWALK ABUTTING CURB
ITEM 503.2



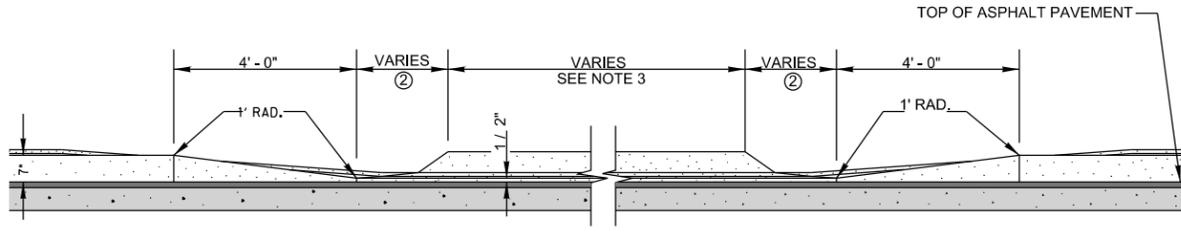
TYPICAL COMMERCIAL DRIVEWAY SECTION
WITH SIDEWALK SEPARATED FROM CURB
ITEM 503.2



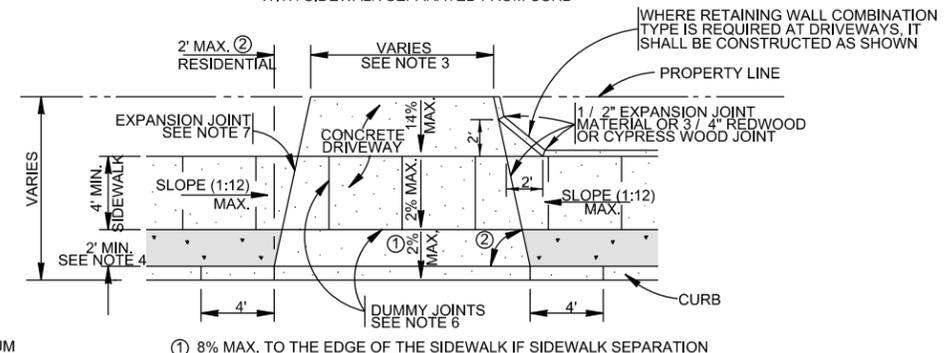
CURB PROFILE AT DRIVEWAY
WITH SIDEWALK ABUTTING CURB



TYPICAL DRIVEWAY PLAN VIEW
WITH SIDEWALK ABUTTING CURB



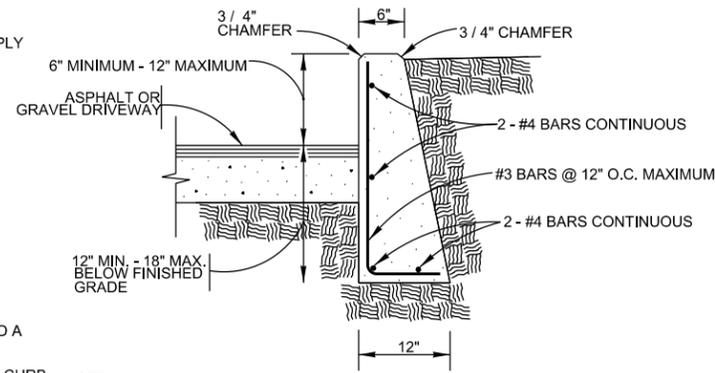
CURB PROFILE AT DRIVEWAY
WITH SIDEWALK SEPARATED FROM CURB



TYPICAL DRIVEWAY PLAN VIEW
WITH SIDEWALK SEPARATED FROM CURB

- CONCRETE DRIVEWAY NOTES**
- ITEM NUMBERS REFER TO CITY OF SAN ANTONIO STANDARD SPECIFICATIONS FOR CONSTRUCTION (JUNE 2008).
 - DRIVEWAY PENETRATION REFERS TO A PORTION OF THE DRIVEWAY THAT MAY BE NECESSARY TO RECONSTRUCT WITHIN PRIVATE PROPERTY TO COMPLY WITH A MAXIMUM DRIVEWAY SLOPE. THIS PORTION OF THE DRIVEWAY SHALL BE PAID FOR UNDER THE FOLLOWING ITEMS AS MAY APPLY:
 - CONCRETE DRIVEWAY PAID FOR UNDER ITEM NO. 503.1 OR 503.2.
 - ASPHALTIC CONCRETE DRIVEWAY PAID FOR UNDER ITEM NO. 503.4 AND SHALL INCLUDE A MINIMUM OF 1" ASPHALT TYPE 'D' & 6" FLEXIBLE BASE
 - GRAVEL DRIVEWAY PAID FOR UNDER ITEM NO. 503.5 AND SHALL INCLUDE A MINIMUM OF 6" FLEXIBLE BASE
 - 7" MINIMUM HEIGHT WILL NOT NECESSARILY OCCUR AT THE PROPERTY LINE. IT MAY OCCUR WITHIN THE RIGHT OF WAY OR WITHIN THE DRIVEWAY PENETRATION ON PRIVATE PROPERTY.
 - THE PROPOSED DRIVEWAY SHOULD MATCH THE EXISTING WIDTH AT THE PROPERTY LINE BUT UNLESS AUTHORIZED BY THE CITY TRAFFIC ENGINEER, THE WIDTH SHALL BE WITHIN THE FOLLOWING VALUES:

TYPE	MIN.	MAX.
RESIDENTIAL	10'	20'
COMM.- ONE WAY	12'	20'
COMM. - TWO WAY	24'	30'
 - FOR LOCAL TYPE "A" STREETS, SIDEWALK SHALL HAVE A MINIMUM WIDTH OF 4' AND IF SEPARATED FROM THE CURB, THE SIDEWALK SHALL BE LOCATED A MINIMUM OF 2' FROM THE BACK OF CURB.
 - FOR OTHER THAN LOCAL TYPE "A" STREETS, THE SIDEWALK SHALL HAVE A MINIMUM WIDTH OF 4' AND SEPARATED A MINIMUM OF 2' FROM THE BACK OF CURB OR, AS AN OPTION, THE SIDEWALK SHALL HAVE A MINIMUM WIDTH OF 6' WHEN LOCATED AT THE BACK OF CURB.
 - DUMMY JOINTS PARALLEL TO THE CURB SHALL BE PLACED WHERE THE SIDEWALK MEETS THE DRIVEWAY. DUMMY JOINTS PERPENDICULAR TO THE CURB, AND WITHIN THE BOUNDARIES OF THE PARALLEL DUMMY JOINTS, SHALL BE PLACED AT INTERVALS EQUAL TO THE WIDTH OF THE SIDEWALK.
 - A MINIMUM OF TWO ROUND AND SMOOTH DOWEL BARS 3/8" IN DIAMETER AND 18" IN LENGTH SHALL BE SPACED 18" APART AT EACH EXPANSION JOINT.
 - SIDEWALK RAMP LENGTHS SHALL BE OF SUFFICIENT LENGTH TO MAINTAIN 8.33% (1:12) MAXIMUM SLOPE. WHERE SIDEWALKS CROSS DRIVEWAYS, SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
 - SIDEWALK RAMP SURFACE SHALL BE BRUSH FINISHED.

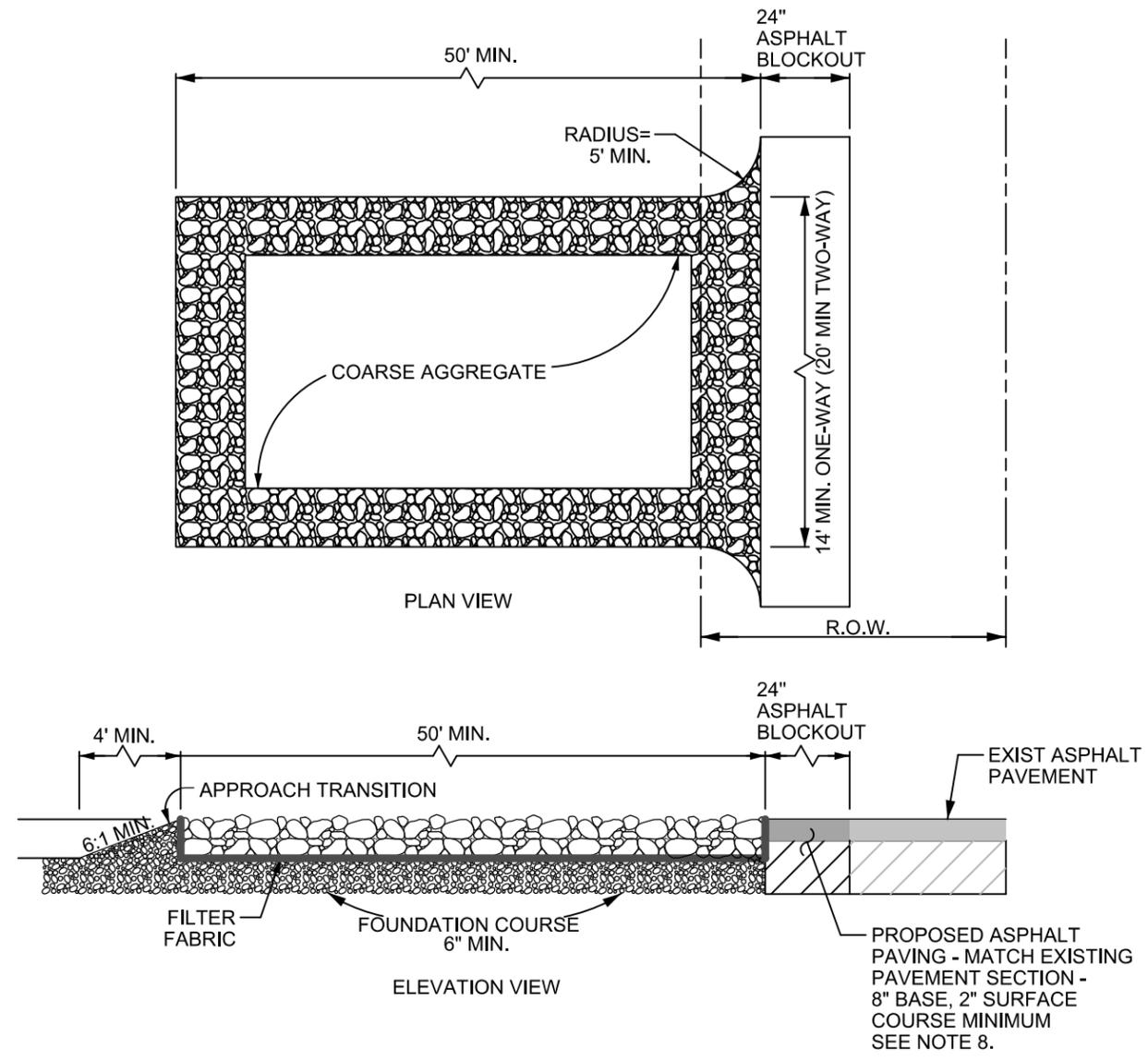


- NOTE:**
- COST OF REINFORCEMENT TO BE INCLUDED IN UNIT COST OF ITEM 307.1.
 - CONCRETE RETAINING WALL COMBINATION TYPE SHALL BE USED FOR CONCRETE DRIVEWAYS.

DRIVEWAY - CONCRETE RETAINING WALL

ON COMPACTED SUBGRADE
ITEM 307.1

TYPE 5: TEMPORARY DRIVEWAY/CONSTRUCTION ENTRANCE



NOTES:

1. THE LENGTH OF THE CONSTRUCTION EXIT SHALL BE AS INDICATED ON THE PLANS, BUT NOT LESS THAN 50'.
2. THE COARSE AGGREGATE SHOULD BE OPEN GRADED WITH A SIZE OF 4" TO 8".
3. THE APPROACH TRANSITION SHOULD BE NO STEEPER THAN 6:1 AND CONSTRUCTED AS DIRECTED BY THE ENGINEER.
4. THE CONSTRUCTION EXIT FOUNDATION COURSE SHALL BE FLEXIBLE BASE, BITUMINOUS CONCRETE, PORTLAND CEMENT CONCRETE OR OTHER MATERIALS APPROVED BY THE ENGINEER.
5. THE CONSTRUCTION EXIT SHALL BE GRADED TO ALLOW DRAINAGE TO A SEDIMENT TRAPPING DEVICE.
6. CONSTRUCT EXITS WITH A WIDTH OF AT LEAST 14 FT. FOR ONE-WAY AND 20 FT. FOR TWO-WAY TRAFFIC FOR THE FULL WIDTH OF THE EXIT, OR AS DIRECTED BY THE ENGINEER.
7. 2' PROPOSED PAVEMENT REPAIR AS NECESSARY. PAVEMENT EDGES WILL BE ASSESSED AND DOCUMENTED BY CITY STAFF PRE- AND POST-CONSTRUCTION TO DETERMINE IF PAVEMENT REPAIR IS NECESSARY DUE TO CONSTRUCTION VEHICLE DAMAGE.
8. IF NECESSARY, PROPOSED PAVEMENT REPAIR WILL MATCH EXISTING PAVEMENT SECTION OR HAVE A MINIMUM SECTION WITH 8" OF FOUNDATION COURSE (BASE) AND 2" OF SURFACE COURSE (HMAC), WHICHEVER IS GREATER. APPROPRIATE TRAFFIC CONTROL CONFORMING TO THE LATEST EDITION OF THE TEXAS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD) WILL BE REQUIRED FOR PAVEMENT REPAIR.

CITY OF BULVERDE, TX	
DRIVEWAY STANDARD TEMPORARY CONSTRUCTION	
	SHEET 5 OF 5